



U.S. Department
Of Transportation

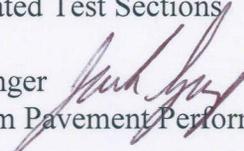
**Federal Highway
Administration**

Memorandum

6300 Georgetown Pike
McLean, Virginia 22101

Subject: **ACTION:** LTPP Directive GO-49
Revised Policy for Monitoring Continuation on
Rehabilitated Test Sections

Date: August 25, 2010

From: Jack Springer 
Long Term Pavement Performance Team

Reply to
Attn of: HRDI-13

To: Dr. Frank Meyer, PM - LTPP North Atlantic Regional Contract
Dr. Frank Meyer, PM - LTPP North Central Regional Contract
Mr. Mark Gardner, PM - LTPP Southern Regional Contract
Mr. Kevin Senn, PM - LTPP Western Regional Contract

Attached is the Long Term Pavement Performance (LTPP) Program Directive GO-49, which provides revised policy for monitoring continuation on rehabilitated test sections. This directive supersedes GO-9 and GO-28. The regions should utilize the directive when they are informed by the State of any upcoming activity at a test section, to determine if monitoring of the test section should continue, or if the test section should go out-of-study. Please ensure that all personnel involved are aware of this new directive.

Should you have any questions or would like to discuss this directive, please do not hesitate to contact me at 202-493-3144.

Attachment (1)

FHWA:HRDI-13:JSpringer:mad:493-3144:08/25/10

File: c:\mdeeny\directive\GO\GO-49dir.doc

cc:

Jonathan Groeger
Directive Binder
LTPP Team
Official file
Chron

LONG TERM PAVEMENT PERFORMANCE PROGRAM DIRECTIVE



For the Technical Direction of the LTPP Program



Program Area: General Operations Directive Number: GO-49
Date: August 25, 2010 Supersedes: GO-9, GO-28
Subject: Revised Policy for Monitoring Continuation on Rehabilitated
Test Sections

Introduction

It is desired that Long-Term Pavement Performance (LTPP) pavement test sections remain in-service for as long as possible so that long-term performance trends can be established. However, since these pavement test sections are located on public roads, some form of modification to the pavement structure will ultimately occur to keep the road in a safe and serviceable condition. Decisions on the timing and choice of applied rehabilitation treatments are the authority of the responsible highway agency.

In order to improve the LTPP program's ability to provide performance information on rehabilitated pavement structures, more rehabilitation test sections are needed. Since LTPP test section recruitment has been completed, the existing LTPP test sections are the only source of additional rehabilitated pavements. Due to the accumulation of information previously collected on these test sections, continuation of monitoring after rehabilitation provides added benefit of observing and understanding pavement performance over the various life cycle phases.

Definitions

The terms maintenance, rehabilitation and reconstruction are used within the LTPP program to classify how various treatments which alter a test section's structure are either documented or treated in the database. This is an important distinction since classification of some of these treatments may differ from highway agency terminology. For example, thin overlays, which some agencies may classify as maintenance, are classified within the LTPP program as rehabilitation since the data forms for overlays of any thickness are the same.

Maintenance - Maintenance activities include seal coats, crack sealing, patching, crack and joint sealing, grinding, milling less than 25-mm deep, and grooving.

Rehabilitation - Rehabilitation activities include overlays and associated pretreatments (patching, milling, joint repair, etc.), inlays (mill and fill), pressure relief joints in PCC pavements, subsealing or undersealing, retrofitted subdrainage, joint load transfer restoration, and shoulder restoration.

Reconstruction – Reconstruction is complete removal and replacement of all bound non-base pavement layers.

General Policy on Monitoring Continuation

Application of maintenance treatments to LTPP test sections does not cause a change in experiment designation and, in general, these test sections will continue to be monitored provided LTPP data sheets documenting the treatment(s) are submitted and notification is provided to the LTPP program sufficiently in advance of the placement of seal coats, or other treatments which alter or hide the pavement surface, so that monitoring measurements can be performed prior to application.

Monitoring of a test section in the LTPP program after rehabilitation will be performed provided the agency notifies the LTPP program in advance of the treatment so that monitoring measurements can be performed prior to construction, the agency agrees with monitoring continuation, and the plan for continued monitoring developed by LTPP regional contractors in concert with the responsible highway agency is accepted by FHWA. All rehabilitation construction activities performed at existing LTPP test sections will be considered for monitoring continuation.

Reconstructed test sections will not be retained in the LTPP program. The LTPP program should be notified sufficiently in advance of reconstruction so that a final round of monitoring measurements can be performed.

Out-of-study Construction Activities

The following construction activities associated with an LTPP test section will place the test section in an out-of-study classification which means that it is no longer a candidate for monitoring continuation:

- Widening of the LTPP test lane.
- Added lane next to the LTPP test lane.
- Addition of tied concrete shoulders to the test lane.
- Application of treatments which hide previous pavement distress without prior notification to the LTPP program. Test sections which had a distress survey performed within six months prior to application of such treatment are still candidates for monitoring continuation.
- Removal of all of the bound non-base pavement layers.
- Application of treatments which result in a non-uniform thickness of bound pavement layers. “Non-uniform” is defined as a variance exceeding 25-mm for at least one third of the test section.
- Construction of an intersection within test section boundaries.
- Addition of exit ramps, entrance ramps, driveways, or other structures which cause new non-uniform traffic movements along the test section.
- Placement of subsurface utilities within the test section boundaries which require cutting through the pavement surface.

Development of Monitoring Continuation Plan

For rehabilitated test sections that the responsible highway agency desires to have LTPP monitoring continued, the LTPP regional contractor is responsible for developing a monitoring continuation plan in concert with the highway agency which includes the following elements and the associated activities will be accomplished:

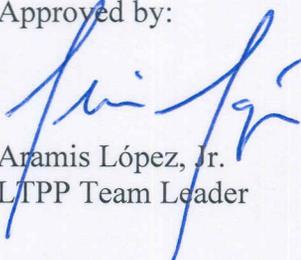
- Collection and reporting of construction data using LTPP data forms.
- Materials sampling and testing plan which address past data deficiencies, provides new and aging tests on existing materials as contained in the SPS Materials Action Plan, and material characterization appropriate to the newly added materials.
- Test section marking and signing.
- Traffic control for pavement monitoring measurements.
- Traffic monitoring plan to address past data deficiencies or to capture changes in traffic loading patterns on the test section.

The monitoring continuation plan should be submitted to FHWA as part of the LTPP RI-2 form provided in LTPP directive GO-14.

Questions concerning this directive should be addressed to the FHWA LTPP Team member responsible for regional operations.

Prepared by: TSSC

Approved by:



Aramis López, Jr.
LTPP Team Leader