



Memorandum

U.S. Department
of Transportation

6300 Georgetown Pike
McLean, Virginia 22101

**Federal Highway
Administration**

Subject: **ACTION:** LTPP Directive D-40, Clarification
of LTPP Manual Distress Data Collection Procedures

Date: November 10, 2004

From: Jack Springer *Jack Springer*
Long Term Pavement Performance Team

Reply to
Attn of: HRDI-13

To: Dr. Frank Meyer, PM - LTPP North Atlantic Regional Contract
Dr. Frank Meyer, PM - LTPP North Central Regional Contract
Mr. Mark Gardner, PM - LTPP Southern Regional Contract
Mr. Kevin Senn, PM - LTPP Western Regional Contract

Attached is Long Term Pavement Performance (LTPP) Program Directive D-40, which presents clarification of LTPP manual distress data collection procedures.

Please make this directive available to all personnel involved in distress data collection and processing. Also, all distress identification manuals for the LTPP program, in use, must be annotated with the appropriate clarifications.

If you have any questions concerning this transmittal, please do not hesitate to call me at (202) 493-3144.

Attachment



LONG TERM PAVEMENT PERFORMANCE PROGRAM DIRECTIVE



For the Technical Direction of the LTPP Program

Program Area: Monitoring

Directive Number: D-40

Date: October 27, 2004

Supersedes: N/A

Subject: Clarification of LTPP Manual Distress Data Collection Procedures

There has been some confusion in differentiating between applications of crack sealant and surface patches as referenced by D-37 or current version of directive. For the purposes of LTPP manual distress surveys, surface patches shall be limited to those where the patching material contains aggregate. Applications of sealant without aggregate are not to be recorded as distresses ACP 7, JCP 15 or CRCP 11. These should be drawn on the map sheets and recorded on the distress survey sheets as distress type 16 (ACP and CRCP) or type 17 (JCP) provided that they exceed 0.1m² in area.

All test sections where it is known that this problem exists should be reviewed. Appropriate changes should be made to all effected map sheets, summaries and database table entries. The database shall be updated with the new information by the last upload in 2005.

Adjacent random cracking is a factor in determining the severity level of several distress types on AC pavement. Such "random cracking" should be considered "adjacent" when it is within 0.3 m of the primary distress. Any other crack should be counted as a separate distress.

For AC, JCP or CRCP pavements, the rater is required to "record the length in meters of longitudinal cracking with sealant in good condition at each severity level." Sealant is NOT considered to be in good condition unless at least 1 continuous meter of sealant in good condition is present on a longitudinal crack. In cases where these cracks are less than 1 m in length, the sealant must be present and in good condition over the entire length of the crack before it can be recorded.

On jointed pavements, a transverse or longitudinal joint spall is defined as "cracking, breaking, chipping, or fraying of slab edges within 0.3 m from the face of the transverse/longitudinal joint". When a crack is within 0.3 m of a joint for only a portion of its length, it should be recorded as a spall only for that portion, so long as that portion is at least 0.3 m long. The

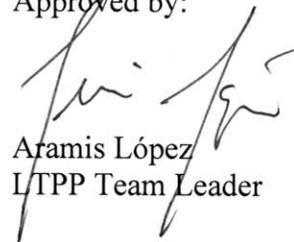
portion of the crack that is greater than 0.3 m from the joint should be recorded as a longitudinal or transverse crack as appropriate.

The minimum recorded area of any pothole shall be 0.1 m^2 . Those potholes that have an area less than 0.1 m^2 but that meet the minimum plan dimension (150 mm) for potholes shall be recorded as 0.1 m^2 in area.

The lateral extent of a test section is defined in the Distress Identification Manual. All distresses within the test section limits are to be recorded on distress map forms and data sheets. As per the Distress Identification Manual, sections begin and end at the stations marked on the pavement. In cases where a transverse crack, or any other distress, falls directly on one of these pavement markings, the rater shall record it consistent with previous surveys.

Prepared by: TSSC

Approved by:



Aramis López
LTPP Team Leader